

507. Warehouse Point Design District (WPDD)

507.1. Purpose:

The purpose of the Warehouse Point Design District (WPDD) is to encourage a fully integrated, mixed use, pedestrian oriented neighborhood. The intent is to incentivize and encourage the cohesive redevelopment and reinvestment of this historic area in a manner which is sustainable, ensures the highest possible economic and social benefits but is also sensitive to the environmental factors associated with the proximity to the Connecticut River.

507.2. Design Goals:

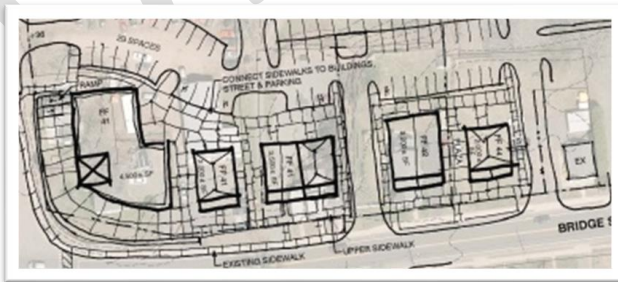
507.2.1.

The WPDD is designed to guide the development of land within the district as a traditional neighborhood. Its design is intended to mirror the development patterns of the nineteenth century and is characterized by the following design elements:

- a. Neighborhoods which are limited in size and oriented toward pedestrian activity
- b. A variety of neighborhood scale commercial uses, services, housing types and public facilities which are interwoven within the neighborhood.
- c. A coordinated transportation system with a hierarchy of appropriately designed facilities for pedestrians, bicycles, public transit and automobiles.
- d. Building architecture and site design which are supportive of the unique character of this area
- e. Compatibility of buildings and site improvements as determined by their arrangement, bulk, form, landscaping, layout and design elements.

507.2.2.

Various aspects of these design goals are demonstrated by the following concepts, the use of which is strongly encouraged when designing new construction or the expansion of existing buildings:



507.3. Applicability:

The provisions of this Section shall apply to any property located within the Warehouse Point Design District (Core, Transition or Periphery) as indicated on the official zoning map and Appendix A.

507.4. Site Development Standards:

Standards for allowable land uses, lot area, coverage, density and yard requirements shall be determined as set forth in this section unless otherwise stated.

a. Streets and Frontage:

Streets and the areas immediately adjacent shall be designed to balance the needs of all users. Streetscape shall consist of wide sidewalks, pedestrian amenities, street trees, street furniture, bike facilities and other improvements to encourage pedestrian activity as demonstrated below.



b. Vehicle Access and Parking and Loading:

1. Parking lots shall be designed in accordance with Section 601.3 unless a separate standard has been established herein.
2. To improve pedestrian safety and traffic flow, sites shall be designed to make use of existing curb cuts and driveway access when possible.
3. To the extent practicable, parking areas shall be located to the side and rear of buildings and not within the front yard. In the case of corner lots, the front yard shall be the portion of the property between the building façade and the most heavily traveled street.
4. Loading docks or areas shall be behind the building and visually screened from view from the road.
5. Shared parking may be allowed by the Commission provided a minimum of fifty percent (50%) of the required parking has been provided and the shared parking spaces are located within five hundred (500) feet.

c. Minimum Parking Requirements:

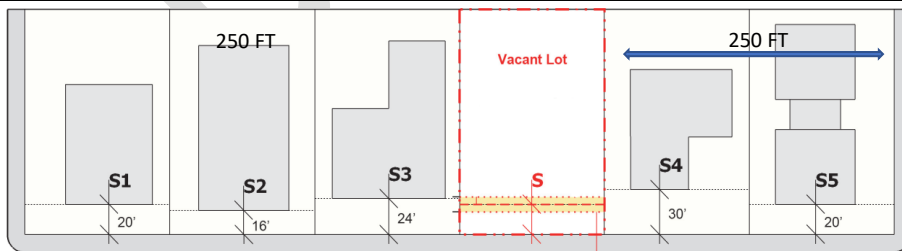
Retail and Service Uses < 10,000SF	3 spaces per 1,000 gross square feet
Retail and Service Uses > 10,000SF	6 spaces to 1,000 gross square feet
Office and Professional Uses	2 spaces per 1,000 gross square feet
Food and Drink Establishments	7 spaces per 1,000 gross square feet
Residential	1.5 spaces per unit
Any Building or Use not listed or greater than 10,000SF	The applicant shall submit a parking plan demonstrating the number of available parking spaces, anticipated parking demands, peak hours of operation and the location of any available off-site parking. The Commission may refer to the parking requirements established in Section 601 to determine the appropriateness of proposed parking.

d. Bulk and Area Requirements:

Zone	Core	Transition	Note
Lot Size			
Frontage	50ft	75ft	
Width	50ft	75ft	
Depth	100ft	100ft	
Setbacks			
Build-to Line ⁽¹⁾	Established or 15	Established or 35	Note 1: Build To line is the line in which the building must be built. Established standard shall be measured using the average for the properties within 250ft of the subject property, on either side (see fig. below). If no identifiable standard, setback shall be 15ft
Side	10ft	15ft	
Rear	15ft	20ft	
Maximum Coverage (2)(3)			Note 2: Maximum coverage may be increased by 10% for sites which contain more than one commercial use Note 3: Site amenities, as defined, shall not count toward impervious coverage
Building	50%	40%	
Total Impervious	70%	60%	
Maximum Height			Note 3: May be increased to 50ft by Special Permit provided a approval has been issued by the PZC for an allowable use.
Total Height(4)	40ft	35ft	
Visual Height	3 Stories	2	
Stories	3	3	
Projections	An open front entrance porch or veranda not more than 10ft in depth or surface patio may project into the front yard setback		

Commented [MD1]: This Section was modified to remove the third zone/category previously shown as "periphery"

Commented [MD2]: This has modified from 35ft to 15ft for the "core" zone to ensure buildings are constructed close to the street/sidewalk.



$$\text{Build To Line Calculation: } S1 + S2 + S3 + S4 + S5 / 5 = 22\text{FT}$$

e. Site Design

Sites shall be designed in accordance with the following criteria and Chapter IV of the Regulations.

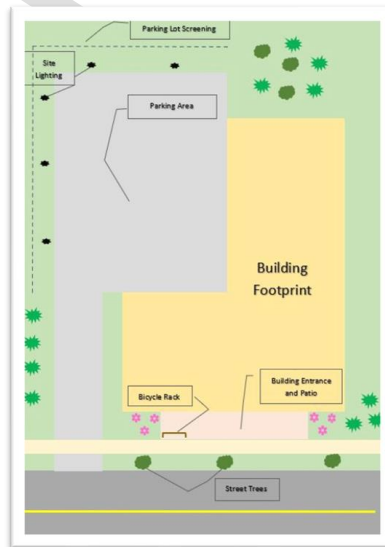
- Sites shall be designed to include Low Impact Development stormwater techniques to the maximum extent practical.
- Given the unique drainage characteristics of this district and the close proximity to the Connecticut River, sites shall be designed to the maximum extent practicable to incorporate a zero-increase stormwater approach for at least fifty percent (50%) of impervious site coverage through the use of on-site infiltration, retention etc. All sites shall be designed to incorporate measures to renovate and improve water quality.
- All lighting shall meet the standards of the International Dark Sky Association and be no higher than sixteen (16) feet
- Landscaping shall include native species, flowering plants, perimeter screening and a mix of deciduous and coniferous/evergreen trees.
- Buffering/visual screening of parking areas to adjacent properties and between residential and commercial uses.
- Any fencing, when required or proposed to run parallel with or immediately perpendicular to the road shall be vinyl, metal or wood. (no chain link)
- When feasible, sidewalks, bike lanes and pedestrian connections to adjacent properties shall be provided or provisions made for future connections.
- All sites shall include a layout which reduces the potential for pedestrian and vehicular conflicts and include a direct entrance from the street.

Commented [MD3]: This language was added to ensure drainage design requirements are not overly burdensome on properties where on-site infiltration is not feasible.

Commented [MD4]: This language was added to require stormwater to be treated/cleaned

f. Site and Building Amenities

- Bicycle Parking
- Street trees which provide shade and grow to appropriate heights
- Streetscape lighting which allows for banners or hanging plants
- Pedestrian benches or other public seating,
- Pocket parks or community focal points
- The installation or improvement of a front entrance porch, veranda or patio

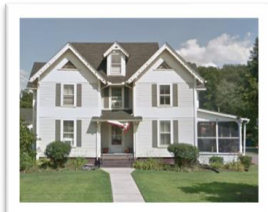




507.5. Building Design

Building Design should include the following materials, and design elements:

- Brick, Vinyl or Wood façade
- Gable or Hipped Dormers
- Ornamental Chimneys
- Shingled Gable or Hipped Roofs
- Shutters, cupolas, window mantles
- Decorative Trim
- Front porches or patios
- Screened parking and garages
- Lamp post, light poles, and other decorate lighting



507.6. Allowable Uses

Property and buildings may be used in accordance with the table below and Section 507.7.

Use Type	Core	Transition	Notes
Residential Uses			
Single Family Detached	x	P	When established as of the date of this Section
Single Family Attached	x	P	
Multi Family (2 to 6)	P	P	
Multi Family (6+)	SUP	SUP	
Residential Above Commercial	P	P	
Food and Drink Establishments			
Sit Down Restaurant	P	P	
Take Out Restaurant	P	P	
Bakeries or Similar	P	P	
Brewery/Distillery/Tasting Room	SUP	SUP	
Mobile Food Vendor	P	P	
Retail and Service Uses			
Retail 0 to 3,000SF	P	P	
Retail 3,000SF to 10,000SF	SUP	SUP	
Retail 10,000SF +	SUP	SUP	
Dry Cleaner/Laundry Services	P	P	
Salon/Barber Shop/Nail Salon	P	P	
Medical Office	P	SUP	
Office and Professional Uses			
Professional Office	P	P	
Financial Institution	P	P	
Veterinary Clinic	P	P	
Daycare	P	P	
Other/Unrelated Uses			
Civic/Governmental Uses	P	P	
Places of Worship	SUP	SUP	
Commercial Agriculture	x	SUP	
Assisted Living/Nursing Home	P	SUP	
Hotels	SUP	SUP	
Accessory Commercial Unit	SUP	SUP	
Multiple Uses Per Parcel	P	P	

Commented [MD5]: This note was added to clarify that existing single-family homes will not be made non-conforming by the adoption of this regulation

X: Prohibited

P: Permitted

SUP: Special Use Permit

507.7. Changes in Use, Site or Building Design

Any new use, change in use, building or site modification shall comply with the criteria of this Section.

507.7.1. Approval Types

The following approval types shall apply to any approval sought within the WPDD.

- A. Zoning Permit (Tier 1):** An administrative zoning approval reviewed by the Zoning Enforcement Officer or Town Planner
- B. Site Plan Review (Tier 2):** An administrative approval reviewed by the Planning & Zoning Commission
- C. Special Use Permit (Tier 3):** An approval reviewed by the Planning & Zoning Commission following a public hearing.

507.7.2. Application Process

- 507.7.2.1. Permitted Use in Existing Building: Tier 1
- 507.7.2.2. Permitted Use in New Building: Tier 2
- 507.7.2.3. Special Permit Use in New Building: Tier 3
- 507.7.2.4. Special Permit Use in Existing Building: Tier 3
- 507.7.2.5. Exterior Changes to any building or site within WPDD including:
 - A. Changes to Building Façade
 - B. Building additions in excess of 500 square feet
 - C. Modifications to site access, loading or changes to parking in excess of ten percent (10%).

507.7.3. Application Compliance Table

Activity	507.4(e)Site Design	507.4(f)Site Amenities	507.5 Building Design
Tier 1 Zoning Permit	Encouraged	Encouraged	Encouraged
Tier 2 Site Plan Review (Sec. TBD)	Required	Required	Encouraged
Tier 3 Special Permit (Sec. TBD)	Required	Required	Required

507.7.4. Property Investment Incentives

507.7.4.1. Expedited Permitting

The application process as defined in Section 507.7.2 may be reduced from Tier 3 to Tier 2 or from Tier 2 to Tier 1 provided the applicant can demonstrate to staff that at least five (5) not required “site amenities” or “building design” criteria established herein are to be added and included within the application materials.

507.7.4.2. Property Rehabilitation Tax Deferral Program (For Discussion)

507.7.4.3. Pre-Approval for Multiple Uses

An applicant may request approval for up to three (3) uses at the time of application, which may be granted by staff, or the Commission in accordance with Section 507.6 provided all necessary site design standards have been satisfied.

507.8 Reduction in Standards

The Commission recognizes that through the adoption of these regulations, unique circumstances may exist which necessitates flexibility to ensure that unintended non-conformities are not created. As such, the Planning & Zoning Commission may grant (with or without a public hearing) a modification to any standard required by Section 507.4 (d) provided the following criteria have been met:

- A. A narrative statement indicating the modification being requested to include an explanation as to why compliance with the published standard is not feasible has been provided.
- B. Such modification does not facilitate the development or expansion of a larger principal building.
- C. Such modification is necessary to provide for the proper development of the site and the inclusion of site amenities as stated in Section 507.4(f)
- D. The approval of such request will not result in the creation of a new or expansion of an existing non-conformity.